



REQUEST FOR PROPOSALS

CHERRY STREET PIER 9 REPAIRS AND HELIPORT PIER 36 REPAIRS

The Delaware River Waterfront Corporation (DRWC) is hereby seeking proposals from experienced firms to provide construction services for the Cherry Street Pier 9 and Heliport Pier 36 Repairs project.

Attached is information relating to submitting a response including specific requirements.

Any **questions** pertaining to this Request will be submitted in writing to Philip Borst, pborst@scungioborstintl.com and Rick Parisi, Rick.Parisi@childseng.com no later than **September 5, 2025**. Answers will be posted on the DRWC website by September 12, 2025 at 5:00 PM

There will be a mandatory **Pre-Bid Meeting** on **August 28, 2025 at 10:00 AM**. The meeting will be on site, at the entrance to Cherry Street Pier.

The **Bids** are due on **September 19, 2025 at 4:00 PM**. Digital copies of the Proposal in PDF format should be sent either as an email attachment or sent as an FTP/Dropbox link to Lizzie Woods at lwoods@drwc.org, Lavelle Young at lyoung@drwc.org and Philip Borst at pborst@scungioborstintl.com.

CHERRY STREET PIER 9 REPAIRS HELIPORT PIER 36 REPAIRS

REQUEST FOR CONSTRUCTION PROPOSALS

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Bid Advertisement

Project: Cherry Street Pier 9 Repairs and Heliport Pier 36 Repairs

DELAWARE RIVER WATERFRONT CORPORATION (DRWC) INVITES PROPOSALS FOR THE CONSTRUCTION OF THE CHERRY STREET PIER 9 REPAIRS AND HELIPORT PIER 36 REPAIRS ALONG DELAWARE RIVER, IN PHILADELPHIA, PA. Bids are invited for the following Contract in connection with the repairs to Cherry Street Pier and Heliport Pier 36.

Questions regarding the project and/or this RFP should be submitted to Philip Borst, pborst@scungiorborstintl.com and Rick Parisi, Rick.Parisi@childseng.com by **September 5, 2025, 5:00 PM**. Responses to all questions will be posted on the DRWC website by September 12, 2025 at 5:00 PM. **Bids** will be received in digital form at: lwoods@drwc.org, lyoung@drwc.org, and pborst@scungiorborstintl.com until **September 19, 2025 at 4:00 PM**. Bids will not be opened publicly.

Owner:

Delaware River Waterfront Corporation

121 N. Columbus Blvd. Philadelphia, PA 19106

Attn: Lizzie Woods, Senior Vice President, Strategic Planning and Real Estate Development
Lavelle Young, Vice President of Operations

Owner's Representative:

Scungio Borst International

Project Manager

4 Manchester Road, Box 203, Media, PA 19065 Tel: 800-940-1937

Attn: Phil Borst and Scott Scungio

Consultants:

Civil Engineer, Structural Engineer

Childs Engineering

4207 Sylon Boulevard, Hainesport, NJ 08036 Tel : 609-261-2268

Attn: Rick Parisi, P.E., Vice President, Project Manager

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I. Definitions

1. **PURPOSE.** This request for proposal (RFP) sets forth the information and requirements for preparing and submitting to the Delaware River Waterfront Corporation (the "Corporation" or "DRWC") proposals for providing Construction Services to the Corporation. This RFP, with any bulletins, contains the only instructions governing the proposal and material to be included therein; a description of the service to be provided; requirements which must be satisfied to be eligible for consideration; general evaluation criteria; and other requirements to be met by each proposal.
2. **ISSUING OFFICE & PROJECT ADMINISTRATOR.** The point of contact for the Corporation regarding the RFP is:

Lizzie Woods, Senior Vice President,
Strategic Planning and Real Estate Development
Delaware River Waterfront Corporation
lwoods@drwc.org

Lavelle Young, Vice President of Operations
Delaware River Waterfront Corporation
lyoung@drwc.org
3. **PROJECT.** DRWC is a nonprofit corporation organized exclusively for the benefit of the City of Philadelphia and its citizens. DRWC acts as the steward of the Delaware River waterfront to provide a benefit to all the citizens and visitors of the city. The fundamental purpose of DRWC is to design, develop and manage the central Delaware River waterfront in Philadelphia between Oregon and Allegheny Avenues. DRWC intends to transform the central Delaware River waterfront into a vibrant destination location for recreational, cultural, and commercial activities for the residents and visitors of Philadelphia. As part of its mission, the Corporation has authorized repairs of Cherry Street Pier, currently serving as a public arts space at 121 S. Christopher Columbus Boulevard, Philadelphia, PA (hereinafter, the "Project").
4. **TYPE OF CONTRACT.** DRWC will enter into one contract: "Construction Form Agreement". DRWC reserves the right to negotiate with bidders whose proposals, as to price and other factors, are considered qualified, responsible, and capable of performing the work required.
5. **SELECTION/REJECTION.** The Corporation may select or reject any and all proposals or may negotiate separately with competing proposers in accordance with Section VI of this RFP.
6. **INCURRING COSTS.** The Corporation is not liable for any costs associated with the RFP incurred by proposers.
7. **AMENDMENTS.** Answers to Contractor's questions or modifications to the contents of the RFP are not official unless issued in writing by the Corporation. All revisions to the RFP will be made by written Bulletin. Such bulletins are the only binding revisions.

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Oral or informal communication are not considered binding.

8. **RESPONSE DATE.** To be considered, **proposals** must be received at the issuing office on or before **September 19, 2025 no later than 4:00 pm. There will be a mandatory Pre-Bid Meeting on August 28, 2025 at 10:00 AM.** Proposals must conform to the content and format requirements described in the RFP. Any other information thought to be relevant, but not applicable to the enumerated categories, should be provided. One (1) copy is to be submitted in electronic format via email. Proposal must be signed by an official authorized to bind the proposer. The proposal must remain valid for no less than thirty (30) calendar days. Late or incomplete proposals will not be accepted regardless of the reason.
9. **CONFIDENTIALITY.** Proposals will be held in confidence by the Corporation, except for the selected proposal. Proposals may be reviewed and evaluated by any person, other than competing proposers, at the discretion of the Corporation. Selection or rejection does not affect that right. All material submitted to the Corporation with the proposal becomes property of the Corporation and will be returned to the proposer only at the Corporation's discretion.
10. **NEWS RELEASES.** News releases and media contacts regarding this project will be made by the Corporation only, unless the Corporation directs otherwise in specific instances.
11. **INSURANCE.** Corporation will require Contractor, its agents, employees, and subcontractors to indemnify the Corporation, waive subrogation and provide insurance coverage types and limits in accordance with the requirements of the Form Construction Agreement. Without limiting the generality of the foregoing, Delaware River Waterfront Corporation, the Philadelphia Redevelopment Authority, the City of Philadelphia, and the Commonwealth of Pennsylvania are to be named as additional insureds on the General Liability Insurance Policy. Also, an endorsement is required stating that the coverage afforded to the above entities and their officers, employees, and agents as additional insureds will be primary to any other coverage available to them. Proposals shall be accompanied by an executed "Undertaking of Insurance" on a standard form provided by the bidder's insurance company stating such company's intention to provide insurance to the bidder in accordance with the insurance requirements of the Form Construction Agreement.
12. **SPECIFICATIONS.** Selected bidder must be able to comply with the Project Specifications attached hereto and incorporated herein as Attachment A ("Technical Specifications").
13. **OWNER'S REPRESENTATIVE.** Scungio Borst International, DRWC's Owner's Representative, will be the primary day-to-day point of contact between Owner, designer, and contractor.

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II. Project Special Conditions

Cherry Street Pier is an active public space and will remain open to the public during all construction services. Required barricades shall be installed and in place no longer than necessary for public safety. Construction impacts to the public and DRWC operations must be carefully mitigated. Jobsite cleanliness is mandatory.

Contractor is notified that no work shall be performed on any Friday, Saturday, or Sunday unless approved by the Owner. Also, no work is to be performed during the weeks between October 5-12, 2025 due to event activities on Cherry Street Pier.

Pier 36 is an active heliport and must remain open during repairs. Construction impacts to the heliport must be carefully mitigated. Jobsite cleanliness is mandatory. All work must be coordinated with the heliport manager.

III. Scope of Services

Cherry Street Pier:

This project involves the repair of Cherry Street Pier 9 along Delaware River in Philadelphia, Pennsylvania. The proposed repairs include the repair of twenty five (25) concrete t-beams and repairing a full depth spall in the deck between the CMU wall and office containers.

The pier was built in 1916 - 1917 and is approximately 100' wide x 535' long. Portions of the original building structure are still intact and in-place on top of the pier. The pier has 27 pile bents, numbered west to east, and spaced at approximately 20' on center. The concrete caps/pier walls are supported on timber piles caps and multiple timber piles (12" to 14" white pine typical). The reinforced concrete t-beam deck/superstructure spans between each of the pier walls/caps below. The outshore (east) face of the pier is a full height, continuous concrete seawall, that is also supported by timber bents and partial length "pony-bents." There are arched openings in the seawall in the tidal zone along the north/south faces between each of the bays, with the exception of bay between Bents 26/27 (which has a full height solid seawall like the east face) and the bays previously used as "wharf drops" (which were spanned over with steel beams supporting a concrete deck with stay-in-place forms and remain fully open). The topside portions of the pier (including the original building structure) were rehabilitated and converted into a mixed use retail/public space that opened in 2018.

Construction work to be performed under this contract shall be as follows:

1. Concrete t-beam repairs: Twenty-five (25) t-beam repairs located under the concrete deck at various locations (see plan). The existing concrete beams to be thoroughly cleaned with hand tools before repairs are started. A notch, approximately 11" high by 9" deep and 4" wide shall be cut into the existing concrete pile caps for the installation of the new C10x20.7 channels located on either side of the t-beam to rest. The two channels shall be placed on each side of the existing concrete t-beams and connected

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with 3/4" diameter high strength bolts (ASTM A325) as shown in the plans. At each bolt location 7/8" diameter holes shall be drilled through the concrete beam.

2. Full depth concrete deck repair: One repair location of the deck located between the CMU wall and office container (see plans). There is a full depth hole through the concrete deck with exposed reinforcement. Repair shall consist of cleaning and removing all loose and deteriorated concrete, and placing new concrete. Existing reinforcement to be cleaned and coated with epoxy.

Heliport Pier 36:

This project involves the repair of Heliport Pier 36 along Delaware River in Philadelphia, Pennsylvania. The proposed repairs include installation of new supports for the deteriorated timber decking between Bents 28 to 34 and sealing the wide crack in the South Hangar building floor.

This structure is currently used as a heliport and supports loads of both aircraft and typical highway vehicles. The pier is approximately 181 feet wide by 272 feet long consisting of timber piles supporting timber pile caps and a timber deck. Above the deck is a concrete seawall along the perimeter of the pier retaining fill. The pier is classified as a "low deck" type structure. There is also an interior portion of the pier which is a timber bulkhead structure with gravel fill noted where visible. The underside of the pier and timber bulkhead along the entire north side of the pier and partially along the west end are inaccessible due to aggradation of the channel bottom and limited clear space beneath the pier.

Construction work to be performed under this contract shall be as follows:

1. Repair of the deteriorated section of timber deck shall be completed with fiberglass reinforced panel and timber as per the plans.
2. Crack repair of 90 L.F. located in the south hanger area. Maximum crack width is 1/4"(±). Existing crack is to be thoroughly cleaned and all loose/deteriorated concrete removed. The crack shall be filled with "Sika Crack Flex Sealant" or equal.

IV. Selection Process

DRWC will review the responses and reserves the right to ask for more information or additional questions of one or more respondents, or to interview one or more respondents. DRWC also may consult other agencies and stakeholders for feedback on the responses to this RFP. DRWC may request revisions to pricing proposals from one or more firms based on refined scope parameters.

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V. Reservation of Rights and Selection Criteria

Reservation of Rights

The DRWC reserves and in its sole discretion may, but shall not be required to, exercise the following rights and options with respect to the proposal submission, evaluation and selection process under this RFP:

- (a) To reject any proposals if, in the DRWC's sole discretion, the proposal is incomplete, the proposal is not responsive to the requirements of this RFP, the respondent does not meet the qualifications set forth in the RFP, or it is otherwise in the DRWC's best interest to do so;
- (b) To supplement, amend, substitute or otherwise modify this RFP at any time prior to selection of one or more respondents for negotiation and to cancel this RFP with or without issuing another RFP;
- (c) To accept or reject any or all of the items in any proposal and award the contract(s) in whole or in part if it is deemed in the DRWC's best interest to do so;
- (d) To reject the proposal of any respondent that, in the DRWC's sole judgment, has been delinquent or unfaithful in the performance of any contract with the DRWC or with others, is financially or technically incapable or is otherwise not a responsible respondent;
- (e) To reject as informal, non-responsive, or otherwise non-compliant with the requirements of this RFP any proposal which, in the DRWC's sole judgment, is incomplete, is not in conformity with applicable law, is conditioned in any way that is unacceptable to the DRWC, deviates from this RFP and its requirements, contains erasures, ambiguities, or alterations, or proposes or requires items of work not called for by this RFP;
- (f) To waive any informality, defect, non-responsiveness and/or deviation from this RFP and its requirements that is not, in the DRWC's sole judgment, material to the proposal;
- (g) To permit or reject at the DRWC's sole discretion, amendments (including information inadvertently omitted), modifications, alterations and/or corrections of proposals by some or all of the respondents following proposal submission;
- (h) To request that some or all of the respondents modify proposals based upon the DRWC's review and evaluation;
- (i) To request additional or clarifying information or more detailed information from any respondent at any time, before or after proposal submission, including information inadvertently omitted by the respondent;
- (j) To inspect and otherwise investigate projects performed by the respondent, whether or not referenced in the proposal, with or without the consent of or notice to the respondent;
- (k) To conduct such investigations with respect to the financial, technical, and other qualifications of each respondent as the DRWC, in its sole discretion, deems necessary or appropriate; and
- (l) To waive and/or amend any of the factors identified in the RFP as pertaining to the respondent's qualifications.

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- (m) To require all work to the underside of the timber deck shall be performed by ADC-I certified commercial divers with at least five (5) years experience performing similar work type work. All work shall be performed in accordance with OSHA and/or Army Corp. of Engineers requirements. A detailed Scope of Work shall be submitted to DRWC to include proposed personnel, listing how work will be performed, and a "Dive Safety Plan".

Contract Negotiation and Award

DRWC reserves and in its sole discretion may, but shall not be required to, exercise the following rights and options with respect to the contract negotiation and award process resulting from this RFP.

DRWC reserves the right to enter into post-submission negotiations and discussions with any one or more respondents regarding price, scope of services, and/or any other term of their proposals, and such other contractual terms as the DRWC may require, at any time prior to execution of a final contract. DRWC may, at its sole election, enter into simultaneous, competitive negotiations with multiple respondents or negotiate with individual respondents seriatim. Negotiations with respondents may result in the enlargement or reduction of the scope of services, or changes in other terms that are material to the RFP and the submitted proposals. In such event, DRWC shall not be obligated to inform other respondents of the changes, or to permit them to revise their proposals in light thereof, unless DRWC, in its sole discretion, determines that doing so is in DRWC's best interest.

In the event negotiations with any respondent(s) are not satisfactory to DRWC, DRWC reserves the right to discontinue such negotiations at any time; to enter into or continue negotiations with other respondents; to enter into negotiations with firms that did not respond to this RFP and/or to solicit new proposals from firms that did not respond to this RFP, including but not limited to negotiations or proposals for components of the System, if any, that are deleted by the DRWC from the successful proposal or the contract resulting from it. DRWC reserves the right not to enter into any contract with any respondent, with or without re-issue of the RFP, if DRWC determines that such is in the DRWC's best interest.

Selection Criteria

Proposals that the DRWC determines, in its sole discretion, are responsive to the RFP will be reviewed by a selection committee designated by the DRWC. The DRWC, in its sole discretion, may require any respondent to make one or more presentations of its proposal to the selection committee, in DRWC offices, at no cost to the DRWC, addressing its ability to satisfy the requirements of this RFP. The DRWC shall not be required, however, to permit any respondent to make such a demonstration.

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Cost to the DRWC is a material factor, but not the sole or necessarily the determining factor in proposal evaluation. The DRWC may, in its sole discretion, award a contract resulting from this RFP to a person or entity other than the responsible and qualified respondent submitting the lowest price. The contract will be awarded to the respondent whose proposal the DRWC determines, in its sole discretion, is the most advantageous to the DRWC and in the DRWC's best interest.

The DRWC, in its sole discretion, may, but shall not be required to, reject without further consideration the proposal of any respondent that has not demonstrated, in the DRWC's sole judgment, that it satisfies the qualifications criteria provided in the RFP. The DRWC reserves the right, in its sole discretion and without notice to respondents, to modify this evaluation procedure as it may deem to be in the DRWC's interest.

VI. Confidentiality, Responsibility, and Disclosure rules

Confidentiality and Public Disclosure:

The successful applicant shall treat all information obtained from DRWC which is not generally available to the public as confidential and proprietary to DRWC. The successful applicant shall exercise all reasonable precautions to prevent any information derived from such sources from being disclosed to any other person. The successful applicant agrees to indemnify and hold harmless DRWC and their respective agents, officials and employees, from and against all liability, demands, claims, suits, losses, damages, causes of action, fines and judgments (including attorney's fees) resulting from any use or disclosure of such confidential and/or proprietary information by the successful applicant or any person acquiring such information, directly or indirectly, from the successful applicant or through this RFQ process.

By submission of a proposal, applicants acknowledge and agree that DRWC is subject to state and local disclosure laws and, as such, are legally obligated to disclose public documents, including proposals, to the extent required hereunder. Without limiting the foregoing sentence, DRWC's legal obligations shall not be limited or expanded in any way by an applicant's assertion of confidentiality and/or proprietary data.

Prior to DRWC's election to release any confidential and proprietary information for the purposes stated in this RFP, applicants shall enter into a Non-Disclosure Agreement with DRWC setting forth the obligations, rights, and remedies related to the disclosure of any confidential and proprietary information during the RFP process.

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Prime Provider/Contractor Responsibility:

The selected applicant will be required to assume responsibility for all services described in their proposals whether or not they provide the services directly. DRWC will consider the selected applicant as the sole point of contact with regard to contractual matters.

Disclosure of Proposal Contents:

Subject to the public disclosure requirements stated above, cost and price information provided in proposals will be held in confidence and will not be revealed or discussed with competitors. All material submitted as part of the RFP process becomes the property of DRWC and will only be returned at DRWC's option. Proposals submitted to DRWC may be reviewed and evaluated by any person other than competing vendors. DRWC retains the right to use any/all ideas presented in any reply to this RFP. Selection or rejection of a proposal does not affect this right.

Non-Discrimination:

The successful vendor, as a condition of accepting and executing a contract with DRWC through this RFP, agree to comply with all relevant sections of the Civil Rights Act of 1964, the Pennsylvania Human Relations Act, Section 504 of the Federal Rehabilitation Act of 1973, and the Americans with Disabilities Act, hereby assuring that:

The provider agency does not and will not discriminate against any person because of race, color, religious creed, ancestry, national origin, age, sex, sexual orientation, handicap or disability in providing services, programs or employment or in its relationship with other provider contractors.

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VII. Cherry Street Pier 9 and Heliport Pier 36 Repairs – Bid Proposal Form

PLEASE NOTE: IN ADDITION TO THIS FORM, A COMPLETE PROPOSAL MUST ALSO INCLUDE: A COMPLETED BID ITEM LIST, A STATEMENT OF QUALIFICATIONS, A COMPLETED SOLICITATION FOR PARTICIPATION AND COMMITMENT FORM (RFP) , COMPLETED DISCLOSURE FORMS, A COMPLETED DRWC CONFLICT OF INTEREST DISCLOSURE CERTIFICATION, AND A STATEMENT OF PROPOSED SUBCONTRACTORS SOLICITATION FOR PARTICIPATION & COMMITMENT FORMS, CAMPAIGN CONTRIBUTION DISCLOSURES AND ACKNOWLEDGEMENTS OF ANY ADDENDA (IF APPLICABLE).

PROJECT TITLE: CHERRY STREET PIER 9 REPAIRS HELIPORT PIER 36 REPAIRS

TO: DELAWARE RIVER WATERFRONT CORPORATION

ATTN: LIZZIE WOODS & LAVELLE YOUNG

VIA EMAIL AT: LWOODS@DWRC.ORG & LYOUNG@DRWC.ORG

PROPOSAL DUE DATE: September 19, 2025 at 4:00 PM

- A. I/We, the undersigned Bidder, hereby agree that the prices herein contained in this Bid Proposal will be used for evaluation purposes in procuring a Contractor.
- B. I/We, the undersigned Bidder, certify that I/we have received, read, and understand the preconstruction Scope of Work and shall perform all necessary preconstruction work required to assure rapid mobilization.
- C. I/We, the undersigned Bidder, certify that I/we have received, read, and understand the Construction Documents and have read and understand the description of this procurement process, all special conditions and any addenda, including the Minutes of the Pre-Bid meeting, and have a good understanding of the project.
- D. I/We, the undersigned Bidder, certify that I/we have the required insurance and project bonding and can provide acceptable insurance certificates if I/we am the successful bidder.

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E. Our Bid for this project as stated here and in our Itemized Bid:

UNIT PRICE FORM					
Item	Description	Unit	Quantity	Unit Bid Price	Amount Bid
Cherry Street Pier 9 Repairs					
1	Cherry Street Pier Mobilization	LS	1		
2	Type 1 Concrete Beam Repair	EA	23		
3	Type 2 Concrete Beam Repair	EA	2		
4	Spall Repair	LS	1		
Heliport Pier 36 Repairs					
5	Heliport Pier 36 Mobilization	LS	1		
6	Timber Deck Repair	LS	1		
7	Crack Repair	LF	90		
BID TOTAL =					

Total Bid Price in words: _____ dollars (\$_____).

IF BIDDER IS AN INDIVIDUAL OR PARTNERSHIP, FORM MUST BE DATED AND SIGNED HERE:
This _____ day of _____, 2025.

Signature of Owner or Partner

Business Name of Bidder

Print Name and Title

Business Address

Telephone Number

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IF BID IS BY A CORPORATION, THIS FORM MUST BE DATED AND SIGNED HERE BY A) THE PRESIDENT OR VICE PRESIDENT AND B) COUNTERSIGNED BY THE SECRETARY, ASSISTANT SECRETARY, TREASURER, OR ASSISTANT TREASURER AND THE CORPORATE SEAL SHALL BE AFFIXED. IF THIS FORM IS NOT SO SIGNED, A DULY CERTIFIED CORPORATE RESOLUTION AUTHORIZING FORM OR EXECUTION USED MUST BE ATTACHED TO THE BID EXECUTED BY THE DULY CERTIFIED INDIVIDUAL.

This _____ day of _____, 2025.

Corporate Seal

Corporate or Business Name of Bidder

Address, Including Zip Code

Telephone Number

Signature of President

Signature of Secretary

Print Name and Title

Print Name and Title

Federal Employer Identification Number: _____

APPENDIX A:

Project Specifications

SPECIAL PROVISIONS

AUTHORIZATION OF CONTRACT

The Contract is authorized by Delaware River Waterfront Corporation, hereinafter referred to as DRWC.

DESCRIPTION OF PROJECT

The Contractor is hereby notified that this bid submittal is for work on Cherry Street Pier and Heliport Pier 36.

Cherry Street Pier work is structural repairs to the deck support beams and placing concrete at a full depth hole in the concrete deck.

Heliport Pier 36 work is structural repairs to the timber low deck structure decking and a crack in the South Hangar building concrete floor slab at Heliport Pier 36.

SPECIFICATIONS TO BE USED

The 2020 PennDOT Publication 408 Specifications (with all current amendments), as amended herein will govern the construction of this Project and the execution of the Contract.

Said Specifications are made a part of these Special Provisions by reference and will not be repeated herein. It is the responsibility of any Bidder and or Contractor to become familiar with these Specifications. Copies of the Standard Specifications are available from the Pennsylvania Department of Transportation.

Whenever any section, subsection, subpart or subheading is amended by such terms as changed to, deleted or added it is construed to mean that it amends that section, subsection, subpart or subheading of the 2020 Specifications unless otherwise noted. Whenever reference to page number is made, it is construed to refer to the 2020 Specifications unless otherwise noted.

Henceforth in this Supplementary Specification, whenever reference to the STATE, COMMISSIONER, DEPARTMENT, ENGINEER, INSPECTOR, RE, ME, or OWNER is made, it is construed to mean DRWC, Scungio Borst International, or Childs Engineering.

GENERAL

Payment to the Contractor will only be made for the Items listed in the proposal. No separate payment will be made for any incidental work. The items within the proposal shall include all work required to complete the project in a complete, acceptable, manner.

INQUIRIES REGARDING THE PROJECT

Submit inquiries regarding discrepancies, errors, or omissions, or concerns regarding the intent or meaning of the Contract to the Owner as follows:

1. **Before Award of Contract.** Submit inquiries by e-mail directly to the following contacts:

Scungio Borst International
4 Manchester Road, Box 203
Media, PA 19065-0203
Phone: 215-292-5550
ATTN: Philip Borst, pborst@scungioborstintl.com

Childs Engineering
4207 Sylon Blvd.
Hainesport, NJ 08036
Phone: 609-261-2268
ATTN: Rick Parisi, P.E., Rick.Parisi@childseng.com

Include the following with each inquiry:

1. Name of the Bidder.
2. Telephone number, fax number, e-mail address, and contact person.
3. Specifics of the inquiry, including anticipated results.

The deadline for submitting inquiries is September 5, 2025 at 5:00 PM.

The engineer will investigate the information provided in the inquiry and, if the engineer determines that a change or response is necessary, the engineer will issue an addendum.

Requests for postponement of bids will not receive a response. The engineer will issue an addendum postponing bids if warranted.

2. **After Award of Contract.** Submit inquiries to the following contact:

Delaware River Waterfront Corporation
121 N Columbus Boulevard, Philadelphia, PA 19106

ATTN: Lizzie Woods, lwoods@drwc.org
Lavelle Young, lyoung@drwc.org

ITEM 1 – MOBILIZATION
[CHERRY STREET PIER]
AS FOLLOWS:

1.1 DESCRIPTION – This work is the assembly and set-up of the general plant required to comply with the contract and with local and State laws and regulations. General plant includes Contractor's office, shops, storage areas, and sanitary or other facilities. This work includes obtaining the required insurance and any other initial items required for the start of the work. This work includes Contractor efforts to transport and position supplies and equipment at the job site at the beginning and completion of all contract items. This work includes the final cleanup of the job site including but not limited to removing all construction debris and waste from the site.

1.2 MATERIAL – Furnish adequate material and furnishings required. The material and furnishings will not be considered a part of the other completed contract items.

1.3 CONSTRUCTION – Construct the required repairs.

1.4 MEASUREMENT AND PAYMENT – LUMP SUM

The owner will pay for mobilization according to the following schedule:

1. One half (50%) of the contract lump sum amount bid for mobilization will be released with the first estimate payable following the Notice to Proceed.
2. An additional 40% of the contract lump sum amount bid for mobilization will be released after 50% of the original total contract amount, including payments for delivered materials but excluding mobilization, is earned.
3. An additional 10% of the contract lump sum amount bid for mobilization will be released after the Project Engineer completes a final walk through.

ITEM 2 & ITEM 3 – BEAM REPAIR
[CHERRY STREET PIER]
AS FOLLOWS:

2.1 DESCRIPTION – This work is for the installation of bolted C-Channels to existing floor beams anchored into the existing pile caps with concrete. Pub408 - Section 704; 1105; 1105.02; 1103.13.

2.2 MATERIAL –

- (a) **C-Channel.** Pub408 - Section 1105 - Fabricated Structural Steel. Steel C-Channels hot dipped galvanized as per Pub408 - Section 1105. with fabrication shop drilled galvanized bolt holes.
- (b) **Bolts.** Pub408 - Section 1105.02 – Bolts. To be ASTM A325 with lock washers and double nuts, hot dipped galvanized.
- (c) **Concrete.** Pub408 - Section 704 - Cement Concrete. To be Speed Crete Blue Line Concrete Repair Mortar or approved equal.

2.3 CONSTRUCTION –

- a) The contractor is notified that two adjacent beams may not be under repair simultaneously.
- b) **C-Channel Bolt Holes.** All bolt holes shall be drilled by the steel fabrication shop and galvanized post drilling.
- c) **Install Topside Traffic Prevention.** The topside of the deck supported each beam during repairs must be barricaded from pedestrian traffic. Coordinate with Cherry Street staff.
- d) **Install Debris Shield.** Prevent large construction debris from entering the river.
- e) **Notch Concrete Pile Cap.** See drawings for dimensions. Use hand tools only. Materials removed during demolition shall be disposed of offsite at a properly designated disposal location.
- f) **Drill Holes in Concrete Beams.** See plans for drilling locations throughout the existing concrete beams.
- g) **Install C-Channels.** Clean existing concrete beam as necessary to install channels flush with beam. Ensure channels are level and snug with the concrete deck. Secure with bolts, lock washers, and double nuts. If beam is damaged and the galvanization layer is breached, corrosion protection must be restored as per Pub408 - Section 1105.
- h) **Install Concrete.** Clean all surfaces prior to placing concrete. Remove all dust and debris.

2.4 MEASUREMENT AND PAYMENT – EACH.

- (A) **Beam Repair.** Per Beam Repair. The unit price includes all material and labor.

ITEM 4 – CONCRETE DECK REPAIR
[CHERRY STREET PIER]
AS FOLLOWS:

3.1 DESCRIPTION – This work is for the full depth concrete deck repair. Pub408 - Section 701 & 709.

3.2 MATERIAL –

(a) **Cement** – Pub408 - Section 701. Shall be Class A Cement.

(b) **Epoxy Coating** – Pub408 – Section 709 – Reinforcing Steel.

3.3 CONSTRUCTION –

(a) **Clean and Prepare Surface.** Remove all loose concrete and debris.

(b) **Wire Brush and Coat Exposed Reinforcing.** Exposed reinforcing steel shall be wire brushed to bare steel and coated with epoxy paint as approved by Pub408 – Section 709.

(c) **Place Cement.** Ensure level surface.

3.4 MEASUREMENT AND PAYMENT – SQUARE FEET

(a) **Concrete Deck Repair.** Per square foot placed.

ITEM 5 – MOBILIZATION
[HELIPORT PIER 36]
AS FOLLOWS:

4.1 DESCRIPTION – This work is the assembly and set-up of the general plant required to comply with the contract and with local and State laws and regulations. General plant includes Contractor's office, shops, storage areas, and sanitary or other facilities. This work includes obtaining the required insurance and any other initial items required for the start of the work. This work includes Contractor efforts to transport and position supplies and equipment at the job site at the beginning and completion of all contract items. This work includes the final cleanup of the job site including but not limited to removing all construction debris and waste from the site.

4.2 MATERIAL – Furnish adequate material and furnishings required. The material and furnishings will not be considered a part of the other completed contract items.

4.3 CONSTRUCTION – Construct the required facilities.

4.4 MEASUREMENT AND PAYMENT – Lump Sum

The owner will pay for mobilization according to the following schedule:

1. One half (50%) of the contract lump sum amount bid for mobilization will be released with the first estimate payable following the Notice to Proceed.
2. An additional 40% of the contract lump sum amount bid for mobilization will be released after 50% of the original total contract amount, including payments for delivered materials but excluding mobilization, is earned.
3. An additional 10% of the contract lump sum amount bid for mobilization will be released after the Project Engineer completes a final walk through.

ITEM 6 – UNDERDECK REPAIR
[HELIPORT PIER 36]
AS FOLLOWS:

5.1 DESCRIPTION – This work is for the repair of the timber underdeck.

5.2 MATERIAL –

- (a) **Structural Timber.** Manufacture sawn timber from Southern Pine with extreme fiber stress in bending in excess of 1,200 pounds per square inch, as assigned according to Southern Pine Inspection Bureau (SPIB). Accept only pieces consisting of sound wood, free from decay.
- Avoid boxed heart timber.
 - Grade Southern Pine according to SPIB inspection rules.
 - Adequately protect structural timber during transport.
 - Accept only kiln dried dimension lumber at 15% average moisture content, and 19% maximum moisture content.

- Accept only solid sawn timbers that have been air seasoned or dried to an average moisture content not exceeding 19%.
- Preservative treat lumber and timber by pressure treatment process according to AWPA Standards UC5B.
- Provide treated lumber and timber products with permanent symbol imprints or documentation to identify the treating company, treatment type, year of treatment, and certification of conformance with AWPA.
- Accept treated structural timber that conforms to appropriate recommended AWPA treatment requirements.

(b) Hardware. Furnish lag screws. Galvanize all hardware.

(c) Galvanizing. Section 1105.02(s). Galvanize hardware after fabrication.

(d) Wood Preservative. Use only Copper Chromated Arsenate (CCA) water-borne preservative with hardwood lumber and timber. Certify as specified by AWPA P5.

(e) Fiberglass Reinforced Panel. See attached Pultex 1500 Standard Fiber Reinforced Polymer Flat Sheets.

5.3 CONSTRUCTION –

(a) Storing and Handling. Store lumber and timber neatly in piles on skids above ground. Protect from exposure to the elements if stored for prolonged periods of time. Store material so that it can be readily inspected.

Handle in a manner to avoid injury or breakage. Handle treated lumber with nylon or other non-damaging rope slings. Do not use cant hooks, peaveys, or other sharp instruments in handling treated timber. Provide corner protection for banded material. The Owner will reject treated lumber unduly injured in handling.

(b) Workmanship. Cut all lumber and timber accurately, and frame to a close fit, to provide for even bearing of joints over the entire contact surfaces. Make joints without shimming. Field drill only as indicated or as allowed.

(c) Fiberglass Reinforced Panel. As per plans and manufacturer.

5.4 MEASUREMENT AND PAYMENT – Lump Sum

This bid item and payment shall include all labor, equipment, and materials as required to satisfactorily complete this item of work.

ITEM 7 – CRACK REPAIR
[HELIPORT PIER 36]
AS FOLLOWS:

6.1 DESCRIPTION – This work is for the repair of a crack in the south hanger.

6.2 MATERIAL –

(a) **Sealant.** Shall be Sikaflex and Crack Flex Sealant or similar.

6.3 CONSTRUCTION –

(a) **Crack Repair.** Follow manufacturer recommended procedure.

(b) **Clean-up.** Dispose of any concrete debris resulting from work.

6.4 MEASUREMENT AND PAYMENT – Linear Feet

This bid item and payment shall include all labor, equipment, and materials as required to satisfactorily complete this item of work.

CHERRY STREET PIER PHOTOGRAPHS

CHERRY STREET PIER



Photo 1: Front face of Cherry Street Pier.



Photo 2: North face of the pier.

CHERRY STREET PIER



Photo 3: Interior of pier looking east.



Photo 4: Interior of pier at the glass enclosure looking east.

CHERRY STREET PIER



Photo 5: Reinforced concrete beam with exposed reinforcement.



Photo 6: Severe deterioration of the bottom of the concrete beam.

CHERRY STREET PIER



Photo 7: Full depth hole in the concrete deck.

HELIPORT PIER 36 PHOTOGRAPHS

HELIPORT PIER 36



Photo 1: Top of Pier looking west.



Photo 2: North face of pier looking east.

HELIPORT PIER 36



Photo 3: Wide crack in the interior of the building.



Photo 4: Broken timber decking.

HELIPORT PIER 36



Photo 5: Missing timber deck members with exposed fill material.

APPENDIX B:

Construction Drawings

PLANS FOR

PIER REHABILITATION

CHERRY STREET PIER

AND

HELIPORT PIER 36

ALONG DELAWARE RIVER

PHILADELPHIA, PENNSYLVANIA

AUGUST 2025




KEY MAP

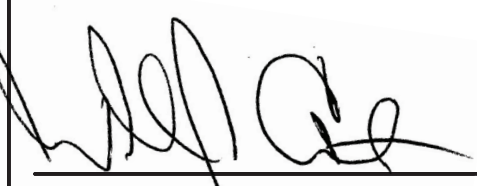
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INDEX OF SHEETS	
SHEET:	DESCRIPTION
01	TITLE PAGE
02	GENERAL NOTES
03	CHERRY STREET PIER SITE PLAN & REPAIR LOCATIONS
04	CHERRY STREET PIER BEAM REPAIR DETAILS
05	CHERRY STREET PIER CONCRETE DECK REPAIR DETAILS
06	HELIPORT PIER 36 REPAIR LOCATION PLAN
07	HELIPORT PIER 36 PROPOSED REPAIR DETAILS

DATE	NO.	REVISIONS	BY
COVER SHEET			
REHABILITATION DESIGN CHERRY STREET PIER AND HELIPORT PIER 36 ALONG DELAWARE RIVER PHILADELPHIA, PENNSYLVANIA			
		4207 SYLON BOULEVARD HAINESPORT, NJ 08036	
SCALE AS SHOWN	DRAWN BY CD	CHECKED BY RP	JOB NO. 3158-25
DATE 08/18/25		DRAWING No. 1 OF 7	




WILLIAM J. CASTLE, P.E.
PA LICENSE NO. 019589

08/18/25
DATE

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GENERAL NOTES:

1. PROVIDE MATERIALS AND PERFORM WORK IN ACCORDANCE WITH LATEST SPECIFICATIONS, PUBLICATION 408, AASHTO/AWS D1.5M/D1.5 BRIDGE WELDING CODE, AND CONTRACT SPECIAL PROVISIONS.
2. THE CONTRACTOR SHALL BE REQUIRED TO PROTECT THEIR WORKERS AT ALL TIMES IN CONFORMANCE WITH ALL APPLICABLE OSHA REGULATIONS. THE CONTRACTOR MUST PERFORM WORK IN A MANNER TO PREVENT ACCIDENTS, INJURY, OR PROPERTY DAMAGE. ALL PRECAUTIONS FOR SAFETY ARE TO BE FOLLOWED. PROVIDE BARRICADES, GUARDRAILS, AND OR OTHER PROTECTION AS NECESSARY AND REQUIRED FOR MAXIMUM SAFETY.
3. THE CONTRACTOR MUST COMPLY WITH ALL APPLICABLE LAWS, ORDINANCES, RULES AND REGULATIONS OF PUBLIC AUTHORITIES AND UTILITY COMPANIES HAVING JURISDICTION.
4. DO NOT CONSIDER ANY OF THE DATA ON THE EXISTING STRUCTURE SUPPLIED IN THE ORIGINAL DESIGN DRAWINGS OR MADE AVAILABLE TO YOU BY DRWC OR ITS AUTHORIZED AGENTS AS POSITIVE REPRESENTATIONS OF ANY OF THE CONDITIONS THAT YOU WILL ENCOUNTER IN THE FIELD.
5. THE CONTRACTOR IS RESPONSIBLE FOR VERIFYING ALL EXISTING DIMENSIONS AND CONDITIONS PRIOR TO THE COMMENCEMENT OF WORK. THE CONTRACTOR SHALL NOTIFY THE ENGINEER IMMEDIATELY OF ANY DISCREPANCIES BETWEEN THE CONTRACT DOCUMENTS AND ACTUAL CONDITIONS, OR OTHER UNFORESEEN CONDITIONS DISCOVERED. THE CONTRACTOR SHALL NOT PROCEED WITH THE WORK UNTIL CONSULTATION AND RESOLUTION WITH THE ENGINEER.
6. THE CONTRACTOR IS TO DISPOSE OF ALL CONSTRUCTION DEBRIS IN A LEGAL MANNER. THE CONTRACTOR IS RESPONSIBLE FOR THE TEMPORARY ON-SITE STORAGE AND THE REMOVAL OF ALL CONSTRUCTION DEBRIS UNLESS INSTRUCTED OTHERWISE BY THE OWNER.
7. THE CONTRACTOR SHALL PERFORM ALL WORK ON SCHEDULE AS NEGOTIATED WITH THE OWNER PRIOR TO THE AWARD OF THE CONTRACT AND THE COMMENCEMENT OF WORK.
8. THE CONTRACTOR IS RESPONSIBLE FOR MAINTAINING THE STRUCTURAL INTEGRITY AND STABILITY OF THOSE PORTIONS OF THE EXISTING STRUCTURE WHICH MAY BE AFFECTED BY THEIR OPERATIONS. ANY DAMAGE TO THE EXISTING STRUCTURE WHICH IS THE RESULT OF THE CONTRACTOR'S OPERATION WILL BE REPAIRED BY THE CONTRACTOR AT NO COST TO THE OWNER.

ESTIMATE AND DISTRIBUTION OF QUANTITIES			
ITEM NO.	DESCRIPTION	UNIT	CONTRACT QTY.
CHERRY STREET PIER			
1	CHERRY STREET PIER MOBILIZATION	LS	1
2	TYPE 1 CONCRETE BEAM REPAIR	EA	23
3	TYPE 2 CONCRETE BEAM REPAIR	EA	2
4	SPALL REPAIR	LS	1
HELIPORT PIER 36			
5	HELIPORT PIER 36 MOBILIZATION	LS	1
6	UNDERSIDE OF DECK REPAIR	LS	1
7	CRACK REPAIR	LF	90

CHERRY STREET PIER NOTES:

1. TWO ADJACENT BEAM REPAIRS SHOULD NOT BE REPAIRED AT THE SAME TIME.
2. THE CONTRACTOR IS TO ENSURE THE DECK AREA ABOVE THE ACTIVE WORK AREA IS SHUT DOWN TO PEDESTRIAN TRAFFIC. THIS INCLUDES THE TIMEFRAME FROM THE DRILLING THE HOLES IN THE BEAMS TO THE FINAL INSTALLATION OF THE CHANNELS. CONTRACTOR SHALL COORDINATE WITH DRWC FOR SHUTTING DOWN THE WALKWAY.
3. CONTRACTOR TO INSTALL DEBRIS SHIELD AS REQUIRED TO STOP ANY LARGE PIECES OF CONCRETE FROM FALLING INTO THE RIVER.
4. CONTRACTOR TO USE MATERIALS AS NOTED IN THESE PLAN, ANY SUBSTITUTIONS FOR "OR EQUAL" MATERIALS WILL HAVE TO BE SUBMITTED FOR APPROVAL BEFORE INSTALLATION.
5. STEEL CHANNELS SHALL BE ASTM A36, GALVANIZED.
6. ALL BOLT HOLES IN THE STEEL MEMBER SHALL BE SHOP DRILLED AND GALVANIZED.
7. ALL BOLTS SHALL BE ASTM A325 HOT DIPPED GALVANIZED UNLESS OTHERWISE NOTED.
8. ALL STRUCTURAL STEEL FABRICATION AND ERECTION SHALL CONFORM TO THE AISC "MANUAL OF STEEL CONSTRUCTION", LATEST EDITION.
9. FOLLOW PENNDOT SPECIFICATION 1105 TO REPAIR ANY DAMAGE TO THE GALVANZED BEAMS.
10. ANY EXPOSED REINFORCING SHALL BE CLEANED WITH A WIRE BRUSH AND COATED WITH EPOXY PAINT AS APPROVED BY PENNDOT SECTION 1040.

CONCRETE NOTES:

1. CHANNEL ENCASEMENT COVER ONLY: USE SPEED CRETE BLUE LINE CONCRETE REPAIR MORTAR.
2. DECK REPAIR ONLY: USE CLASS A CEMENT CONCRETE EXCEPT AS NOTED.
3. A HIGHER CLASS CONCRETE MAY BE SUBSTITUTED FOR A LOWER CLASS CONCRETE AT NO ADDITIONAL COST TO DRWC, IF APPROVED BY THE ENGINEER.
4. ALL CONCRETE SHALL BE MIXED, PROPORTIONED, CONVEYED AND PLACED IN ACCORDANCE WITH THE LATEST ACI 301 AND ACI 318, UNLESS OTHERWISE SPECIFIED.
5. ALL CONCRETE AGGREGATES AND ADMIXTURES SHALL BE IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS.
6. MIX WATER FOR CONCRETE SHALL BE CLEAN, FRESH, AND POTABLE.
7. THE CONCRETE SPALL AREA IS TO CLEANED OF ANY LOOSE OR SOFT CONCRETE. THE SPALL IS APPROXIMATELY 1 SF, HOWEVER EXACT LIMITS ONCE CLEANED IS UNKNOWN.

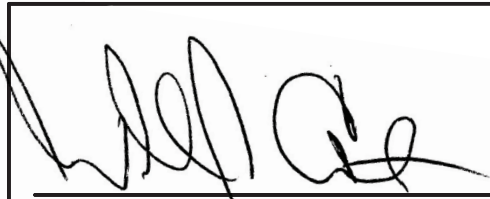
HELIPORT PIER 36 NOTES:


1. CONTRACTOR TO USE MATERIALS AS NOTED IN THESE PLAN, ANY SUBSTITUTIONS FOR "OR EQUAL" MATERIALS WILL HAVE TO BE SUBMITTED FOR APPROVAL BEFORE INSTALLATION.
2. ALL HARDWARE SHALL BE HOT DIPPED GALVANIZED UNLESS OTHERWISE NOTED.
3. ALL TIMBER ELEMENTS SHALL BE SOUTHERN YELLOW PINE. TIMBERS SHALL BE PRESERVED WITH CHROMATED COPPER ARSENATE (CCA) WITH A RETENTION OF 2.5 PCF IN ACCORDANCE WITH AMERICAN WOOD PRESERVERS ASSOCIATION (AWPA) STANDARDS UCSB.
4. FRP PANEL TO BE PULTEXT 1500 SERIES FIBER REINFORCED POLYMER FLAT SHEETS OR EQUAL.
5. CRACK REPAIRED WITH SIKAFLEX AND CRACK FLEX SEALANT OR EQUAL.

ABBREVIATIONS:

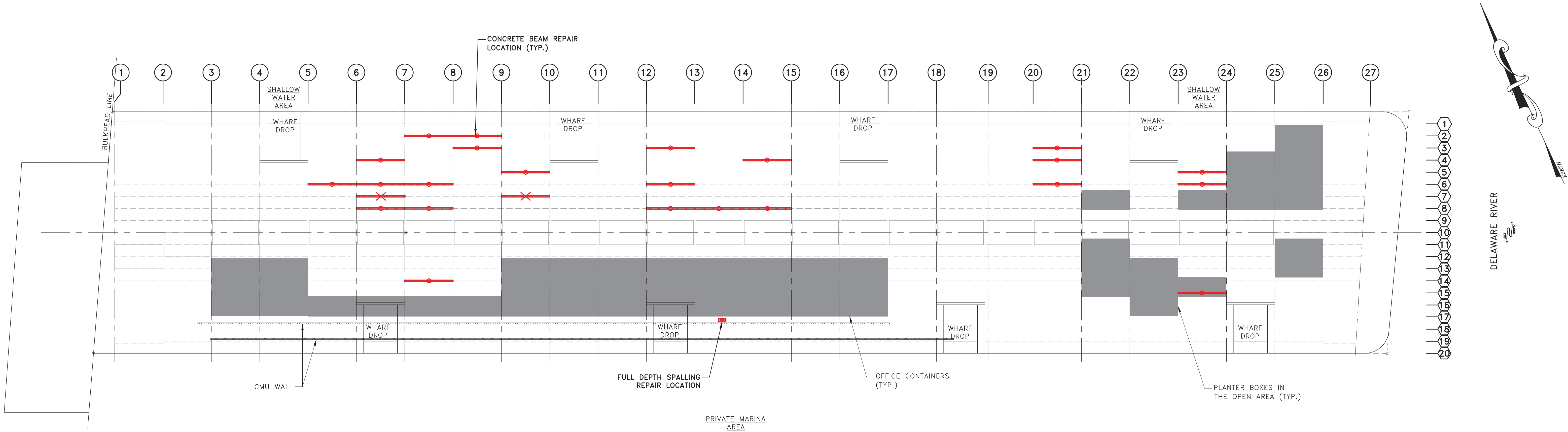
B	- BASELINE	SF	- SQUARE FEET
CL	- CENTERLINE	SY	- SQUARE YARDS
CLR	- CLEAR	STA.	- STATION
CY	- CUBIC YARDS	C-C	- CENTER-TO-CENTER
EA	- EACH	EXIST.	- EXISTING
LF	- LINEAR FEET	PROP.	- PROPOSED
LS	- LUMP SUM	TYP.	- TYPICAL
LBS	- POUNDS	REHAB.	- REHABILITATION
NTE	- NOT TO EXCEED		




WILLIAM J. CASTLE, P.E.
PA LICENSE NO. 019589
08/18/25
DATE

DATE	NO.	REVISIONS			BY
GENERAL NOTES					
REHABILITATION DESIGN CHERRY STREET PIER AND HELIPORT PIER 36 ALONG DELAWARE RIVER PHILADELPHIA, PENNSYLVANIA					
		CHILDS ENGINEERING		4207 SYLON BOULEVARD HAINESPORT, NJ 08036	
SCALE AS SHOWN	DRAWN BY CD	CHECKED BY RP	JOB NO. 3158-25	DATE 08/18/25	DRAWING No. 2 OF 7

NOT FOR CONSTRUCTION
FOR BIDDING PURPOSES ONLY



REPAIR LOCATION PLAN

SCALE: 1" = 20'-0"

LEGEND:

- ① - PIER OR BENT NUMBER
- ① - BEAM LINE
- - EXISTING CMU WALL
- - EXISTING CONTAINERS OR PLANTERS ON THE PIER DECK
- ▨ - DECK REPAIR LOCATION
- - TYPE 1 BEAM REPAIR LOCATIONS
- X— - TYPE 2 BEAM REPAIR LOCATIONS

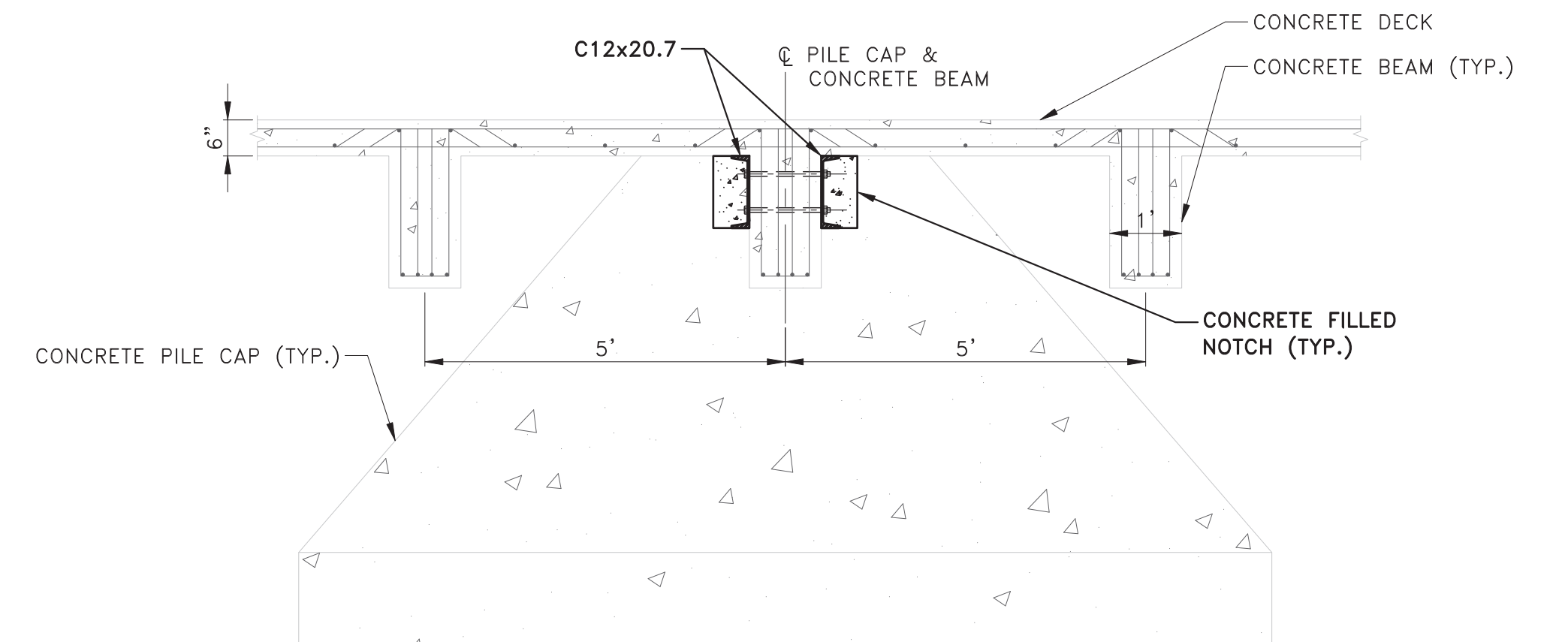
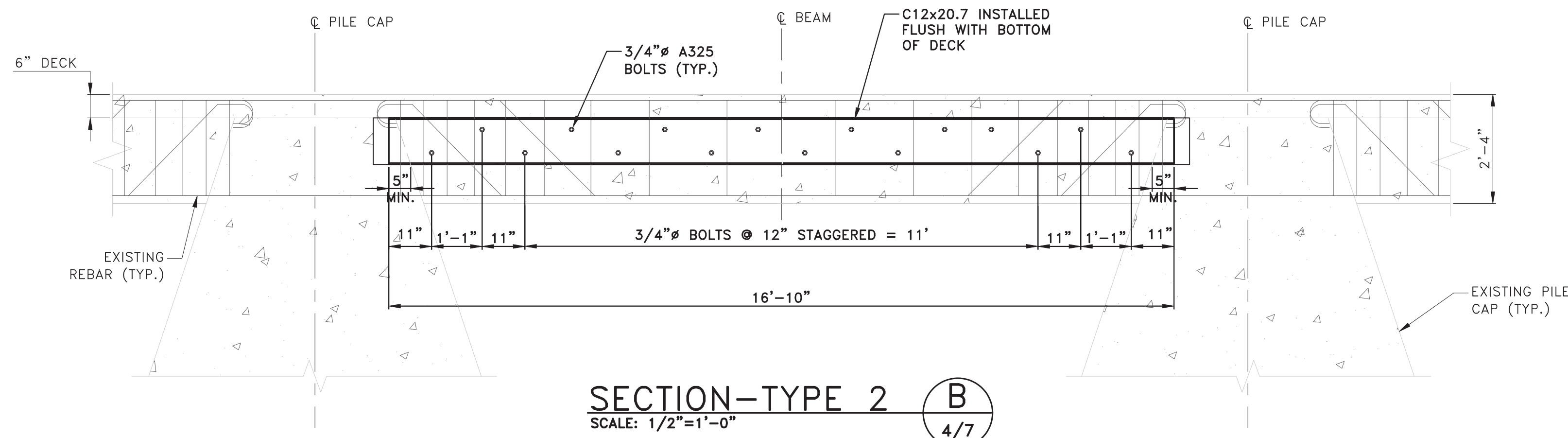
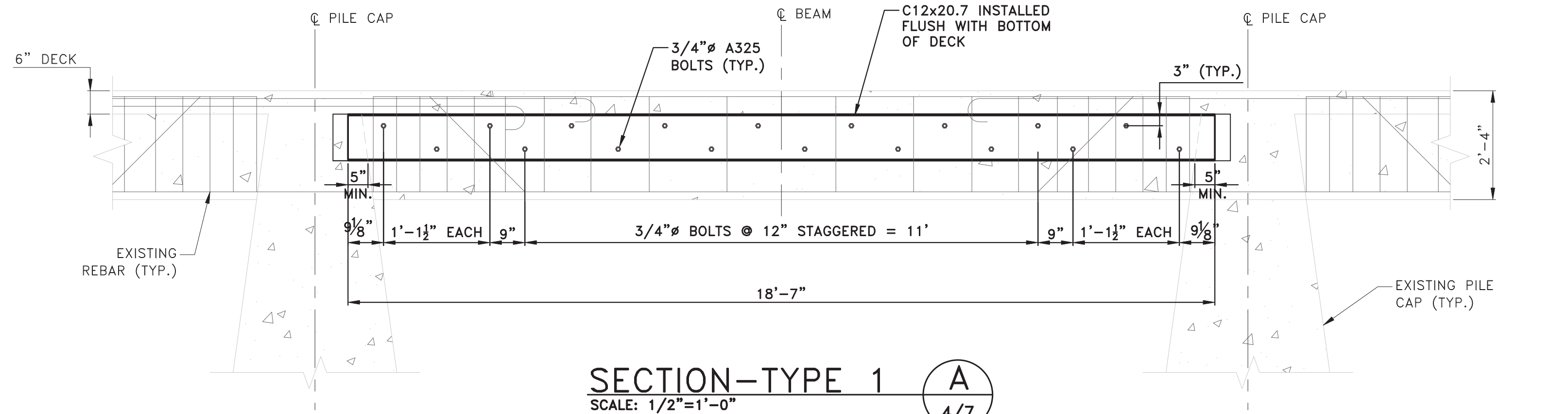
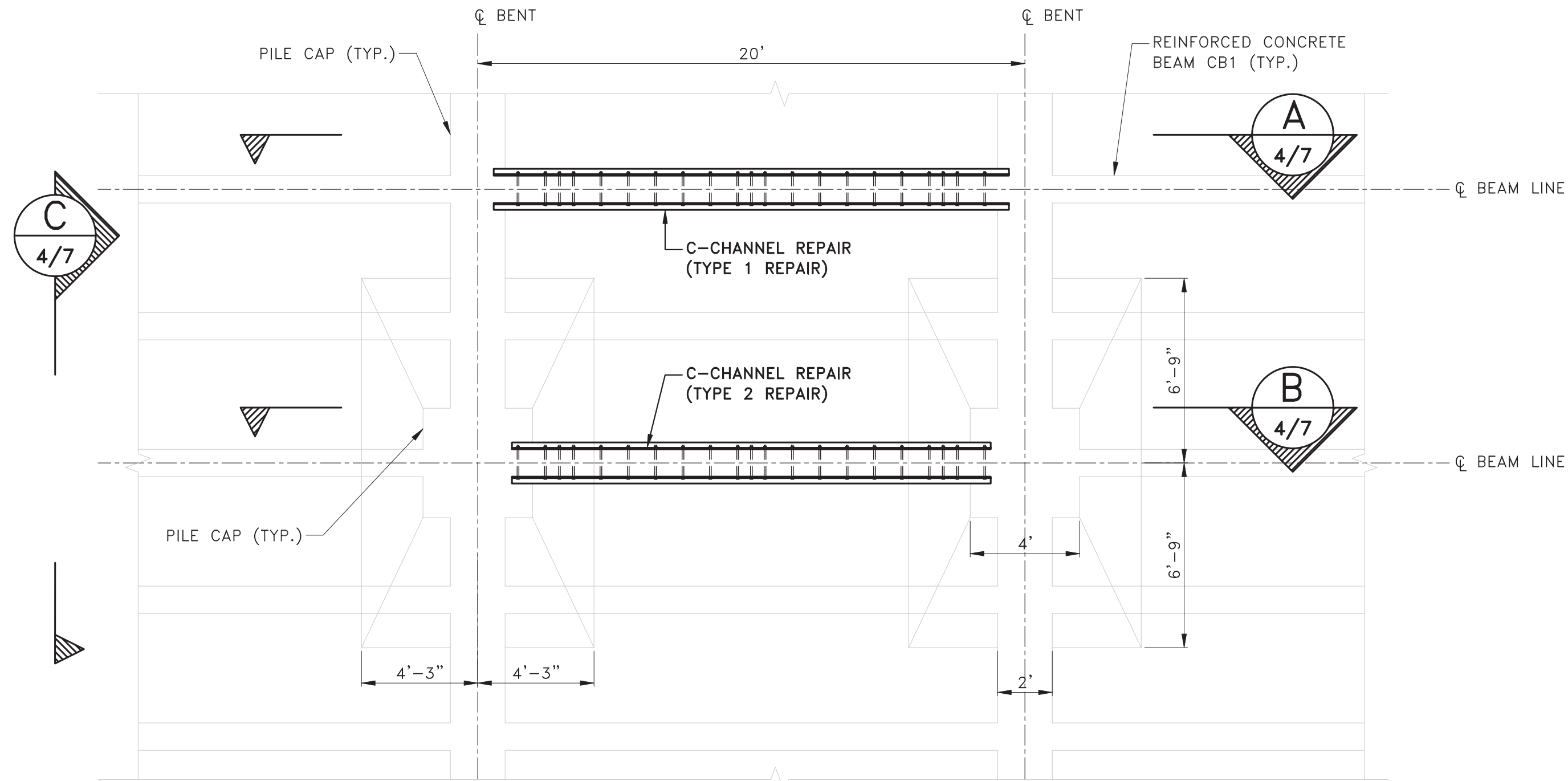
BEAM REPAIR LOCATIONS			
REPAIR NO.	BENT NO.	BEAM LINE NO.	REPAIR TYPE
1	5-6	6	TYPE 1
2	6-7	4	TYPE 1
3	6-7	6	TYPE 1
4	6-7	7	TYPE 2
5	6-7	8	TYPE 1
6	7-8	2	TYPE 1
7	7-8	6	TYPE 1
8	7-8	8	TYPE 1
9	7-8	14	TYPE 1
10	8-9	2	TYPE 1
11	8-9	3	TYPE 1
12	9-10	5	TYPE 1
13	9-10	7	TYPE 2
14	12-13	3	TYPE 1
15	12-13	6	TYPE 1
16	12-13	8	TYPE 1
17	13-14	8	TYPE 1
18	14-15	4	TYPE 1
19	14-15	8	TYPE 1
20	20-21	3	TYPE 1
21	20-21	4	TYPE 1
22	20-21	6	TYPE 1
23	23-24	5	TYPE 1
24	23-24	6	TYPE 1
25	23-24	15	TYPE 1

DATE	NO.	REVISIONS	BY
CHERRY STREET PIER REPAIR LOCATION PLAN			
REHABILITATION DESIGN CHERRY STREET PIER ALONG DELAWARE RIVER PHILADELPHIA, PENNSYLVANIA			
<div><div><div><div><div><div></div><div>CHILDS</div><div>ENGINEERING</div></div></div><div>4207 SYLON BOULEVARD HAINESPORT, NJ 08036</div></div></div><div><div>DATE</div><div>08/18/25</div></div><div><div>SCALE</div><div>AS SHOWN</div></div><div><div>DRAWN BY</div><div>CD</div></div><div><div>CHECKED BY</div><div>RP</div></div><div><div>JOB NO.</div><div>3158-25</div></div><div><div>DATE</div><div>06/10/25</div></div><div><div>DRAWING NO.</div><div>3 OF 5</div></div></div>			

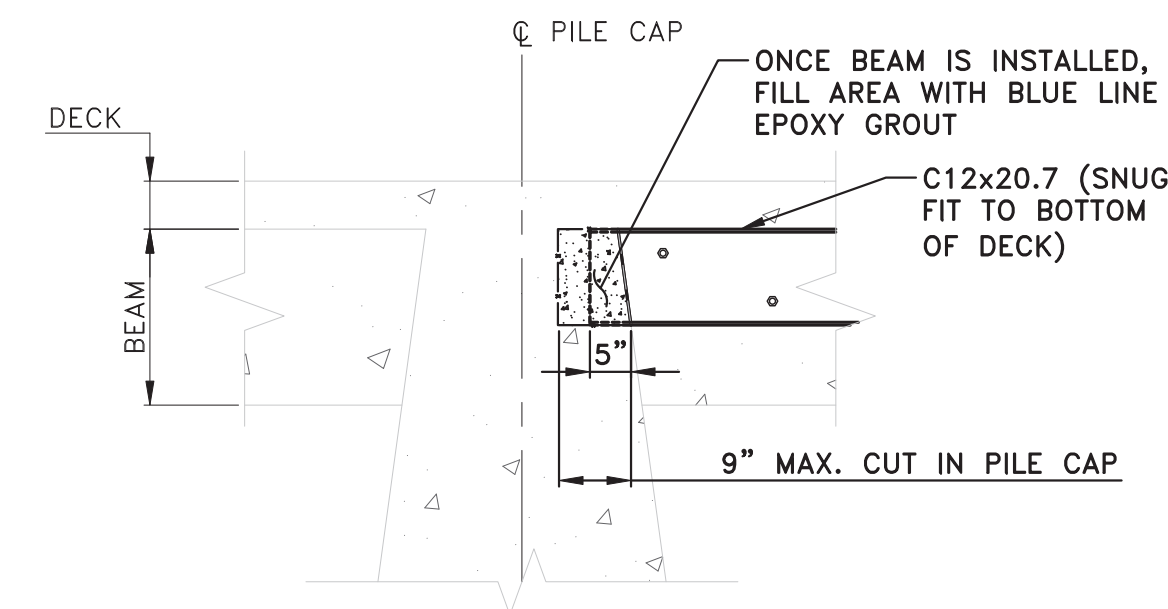


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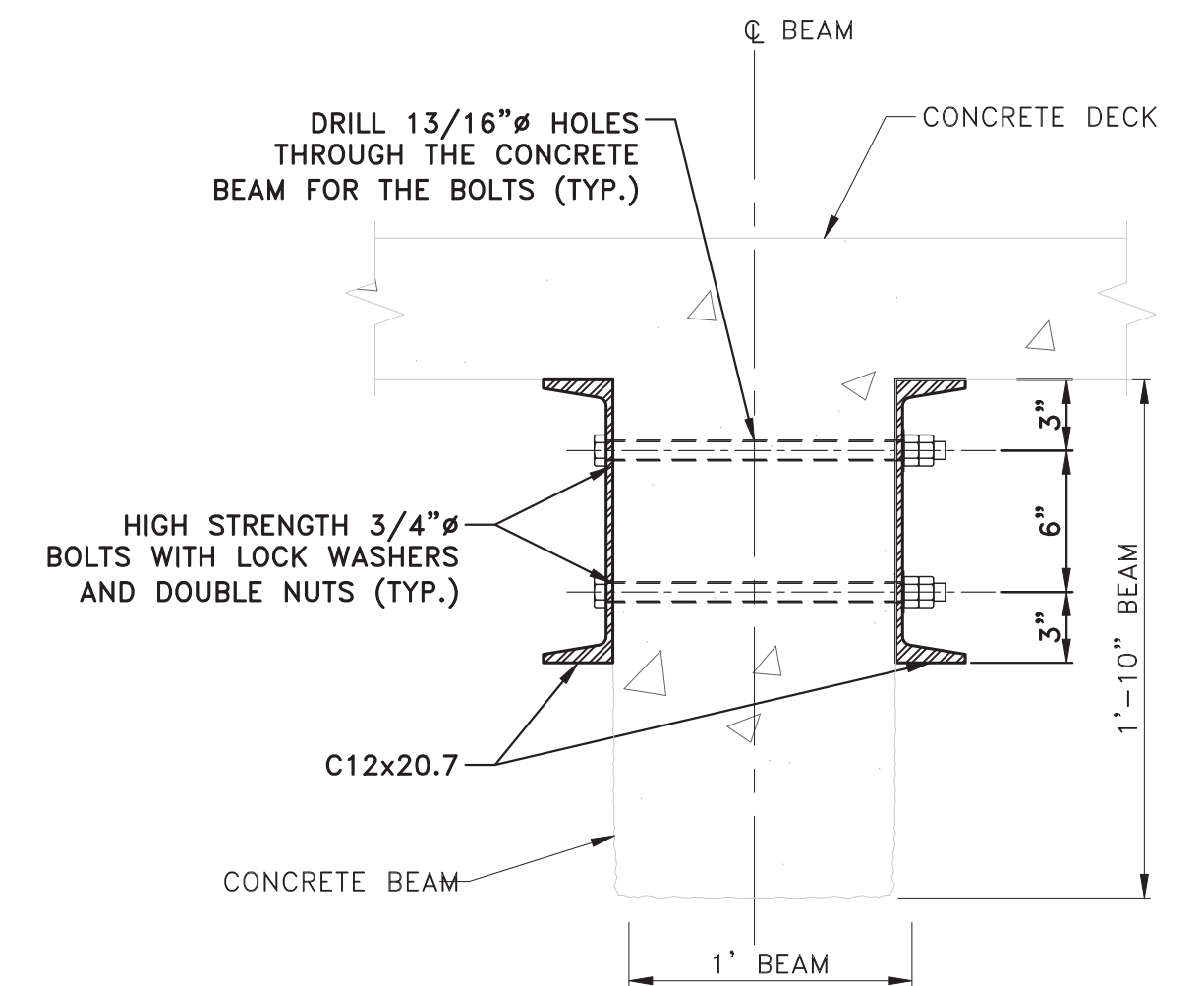
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SECTION C
SCALE: 1/2"=1'-0"




PILE CAP CONNECTION DETAIL
SCALE: 1/2" = 1'-0"

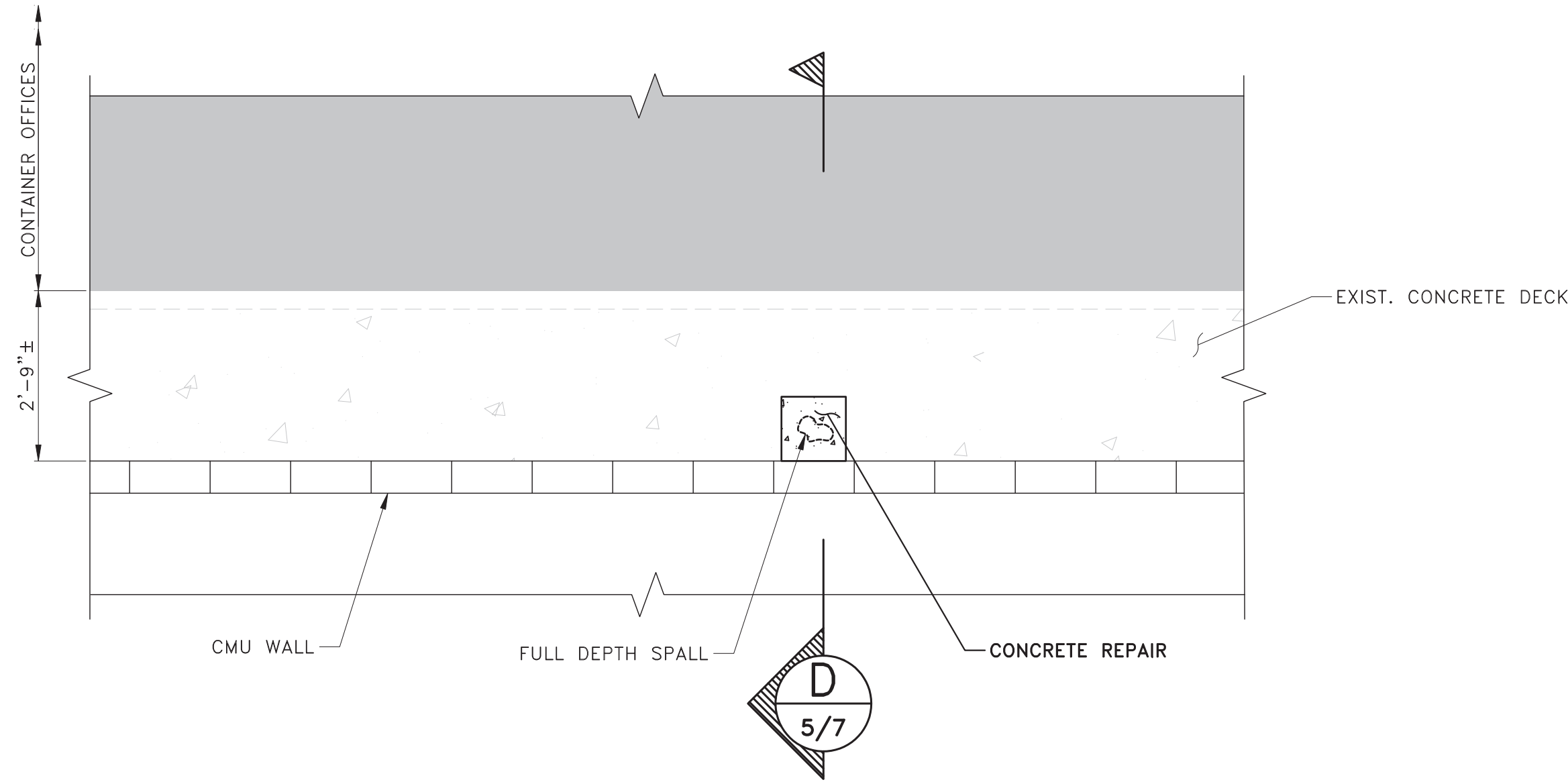


REPAIR CONNECTION DETAIL
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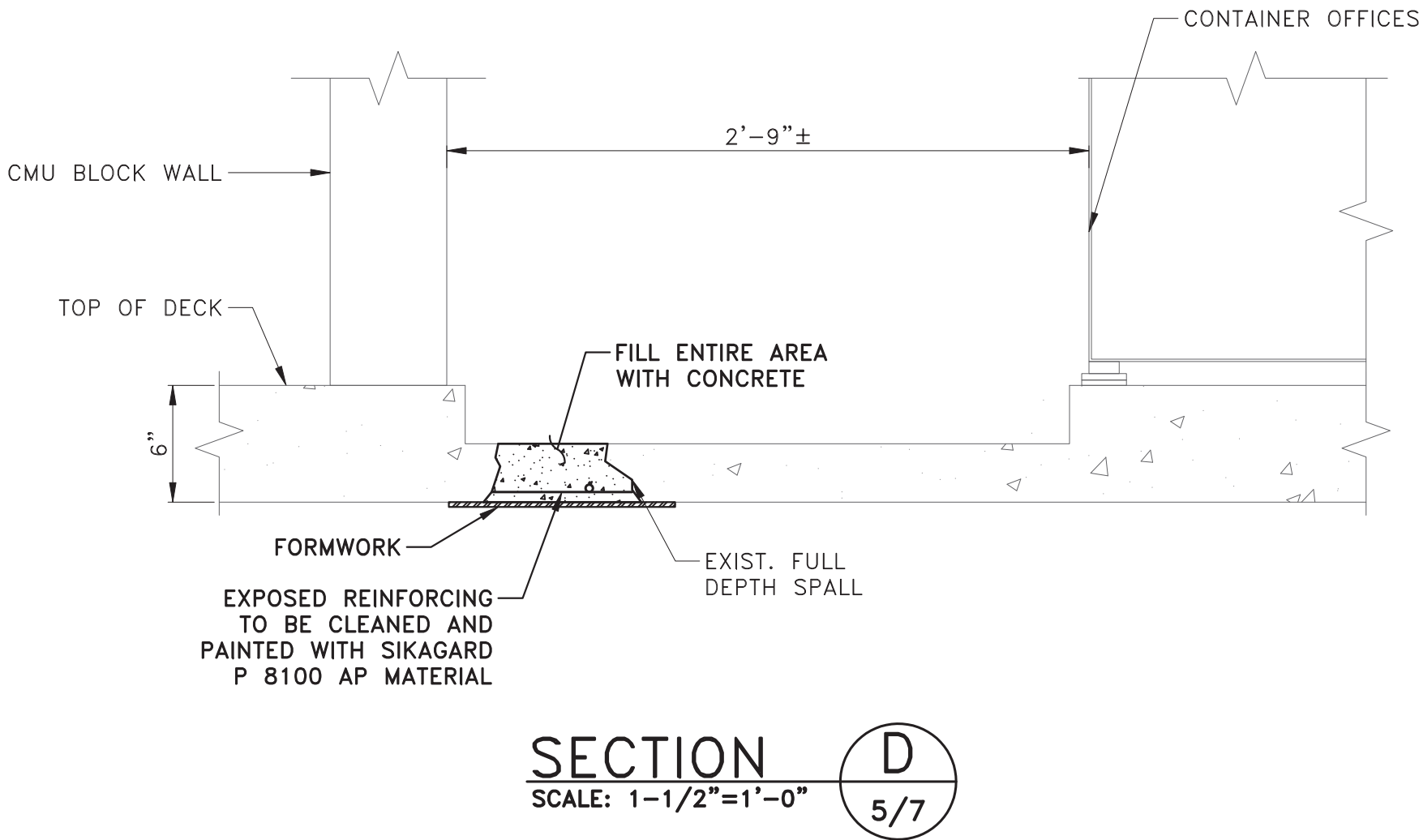



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PA LICENSE NO. 019589
DATE 08/18/25

DATE	NO.	REVISIONS	BY
CHERRY STREET PIER BEAM REPAIR DETAILS			
REHABILITATION DESIGN CHERRY STREET PIER AND HELIPORT PIER 36 ALONG DELAWARE RIVER PHILADELPHIA, PENNSYLVANIA			
 4207 SYLON BOULEVARD HAINESPORT, NJ 08036			
SCALE AS SHOWN	DRAWN BY CD	CHECKED BY RP	JOB NO. 3158-25
DATE 08/18/25		DRAWING NO. 4 OF 7	



**PARTIAL PLAN VIEW
CONCRETE DECK REPAIR**
SCALE: 1/2" = 1'-0"

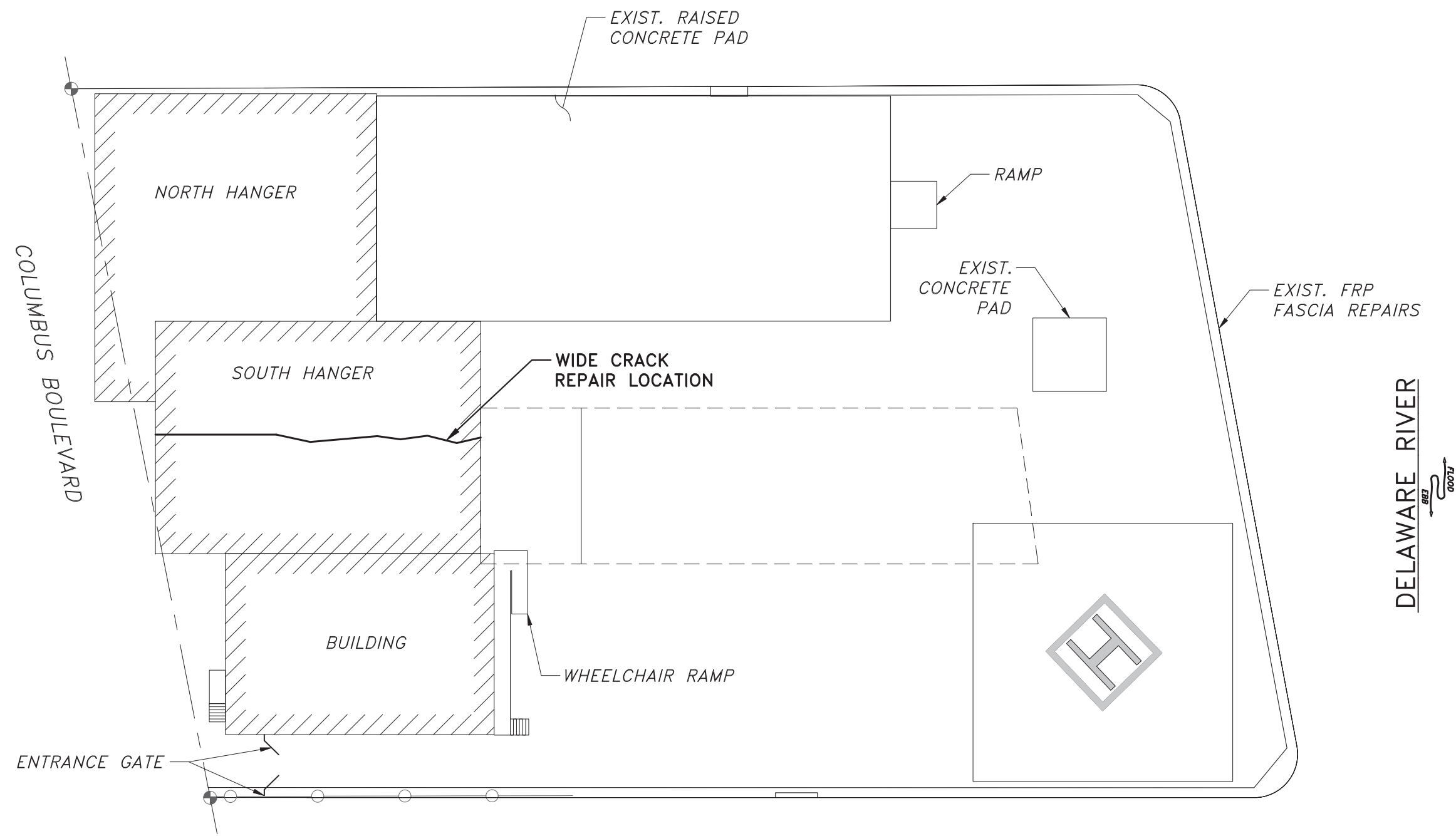


DATE	NO.	REVISIONS	BY
CHERRY STREET PIER CONCRETE DECK REPAIR DETAILS			
REHABILITATION DESIGN CHERRY STREET PIER AND HELIPORT PIER 36 ALONG DELAWARE RIVER PHILADELPHIA, PENNSYLVANIA			
<div>  <div> CHILDS ENGINEERING </div> </div> <div> 4207 SYLON BOULEVARD HAINESPORT, NJ 08036 </div>			
SCALE AS SHOWN	DRAWN BY CD	CHECKED BY RP	JOB NO. 3158-25 DATE 08/18/25 DRAWING No. 5 OF 7



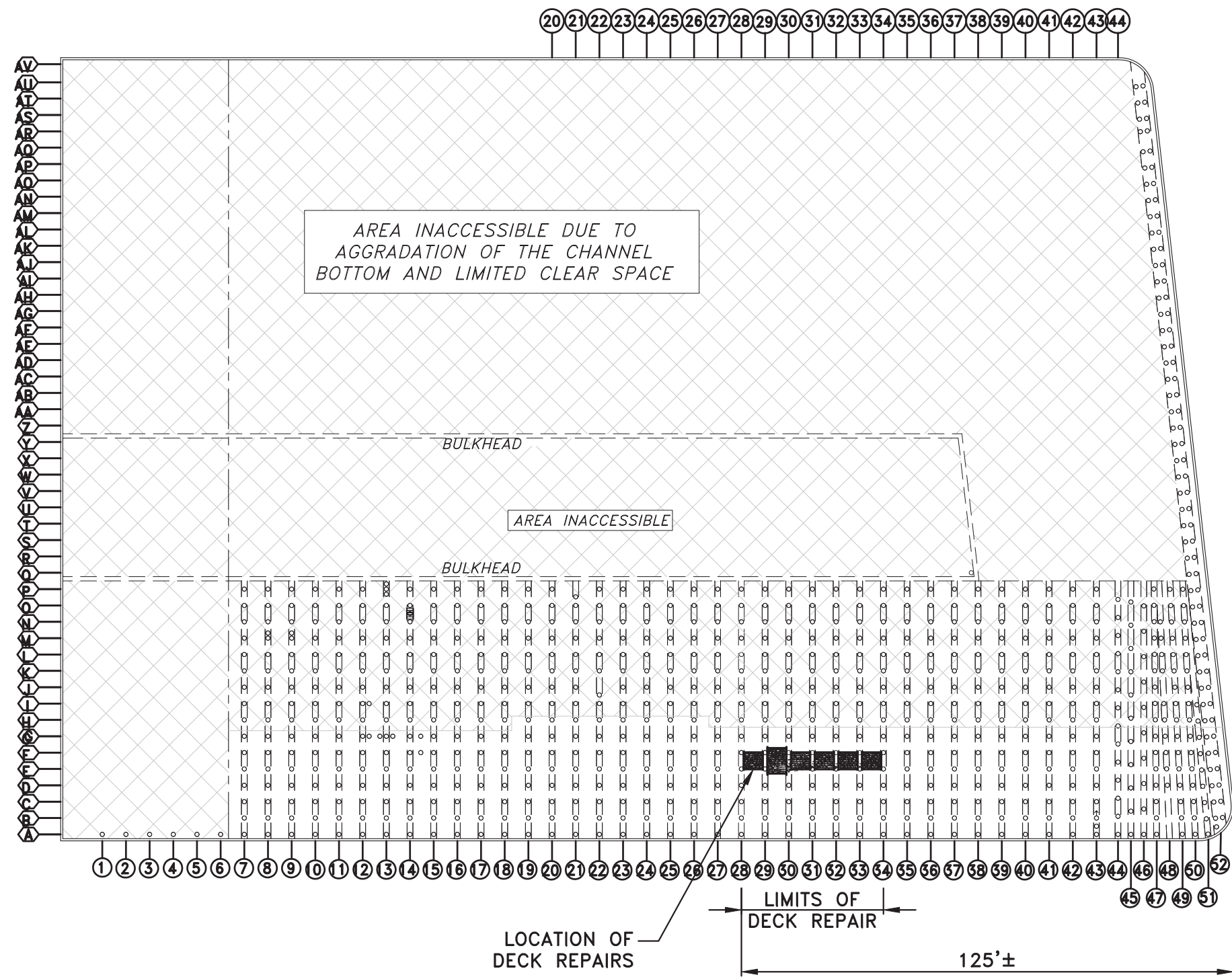

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PA LICENSE NO. 19589
08/18/25
DATE

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DECK PLAN

SCALE: 1" = 30'-0"




PILE LOCATION PLAN

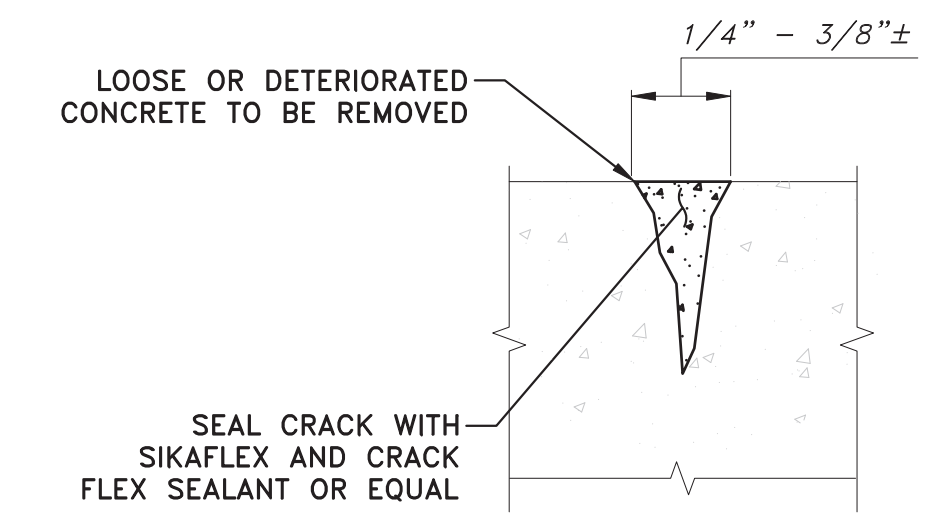
SCALE: 1" = 30'-0"

- LEGEND:
- ① - DENOTES BENT NUMBER
 - △ - DENOTES PILE NUMBER
 - - DENOTES PILE
 - ⊙ - DENOTES BATTERED PILE
 - ⊠ - DENOTES INACCESSIBLE AREA
 - - DENOTES PROPOSED DECK REPAIR AREA

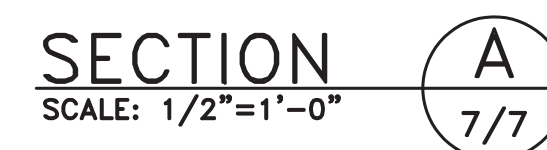



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WILLIAM J. CASTLE, P.E.
PA LICENSE NO. 19589
08/18/25
DATE

DATE	NO.	REVISIONS	BY
HELIPORT PIER 36 REPAIR LOCATION PLAN			
REHABILITATION DESIGN CHERRY STREET PIER AND HELIPORT PIER 36 ALONG DELAWARE RIVER PHILADELPHIA, PENNSYLVANIA			
 CHILDS ENGINEERING 4207 SYLON BOULEVARD HAINESPORT, NJ 08036			
SCALE AS SHOWN	DRAWN BY CD	CHECKED BY RP	JOB NO. 3158-25
DATE 08/18/25		DRAWING No. 6 OF 7	



PROPOSED CRACK
REPAIR DETAIL
SCALE: N.T.S.




 08/18/25
 DATE
 WILLIAM J. CASTLE, P.E.
 PA LICENSE NO. 819589

DATE	NO.	REVISIONS			BY
<p align="center"> HELIPORT PIER 36 PROPOSED REPAIR DETAILS </p>					
<p align="center"> REHABILITATION DESIGN CHERRY STREET PIER AND HELIPORT PIER 36 ALONG DELAWARE RIVER PHILADELPHIA, PENNSYLVANIA </p>					
		CHILDS ENGINEERING		4207 SYLON BOULEVARD HAINESPORT, NJ 08036	
SCALE	DRAWN BY	CHECKED BY	JOB NO.	DATE	DRAWING NO.
AS SHOWN	CD	RP	3158-25	08/18/25	7 OF 7